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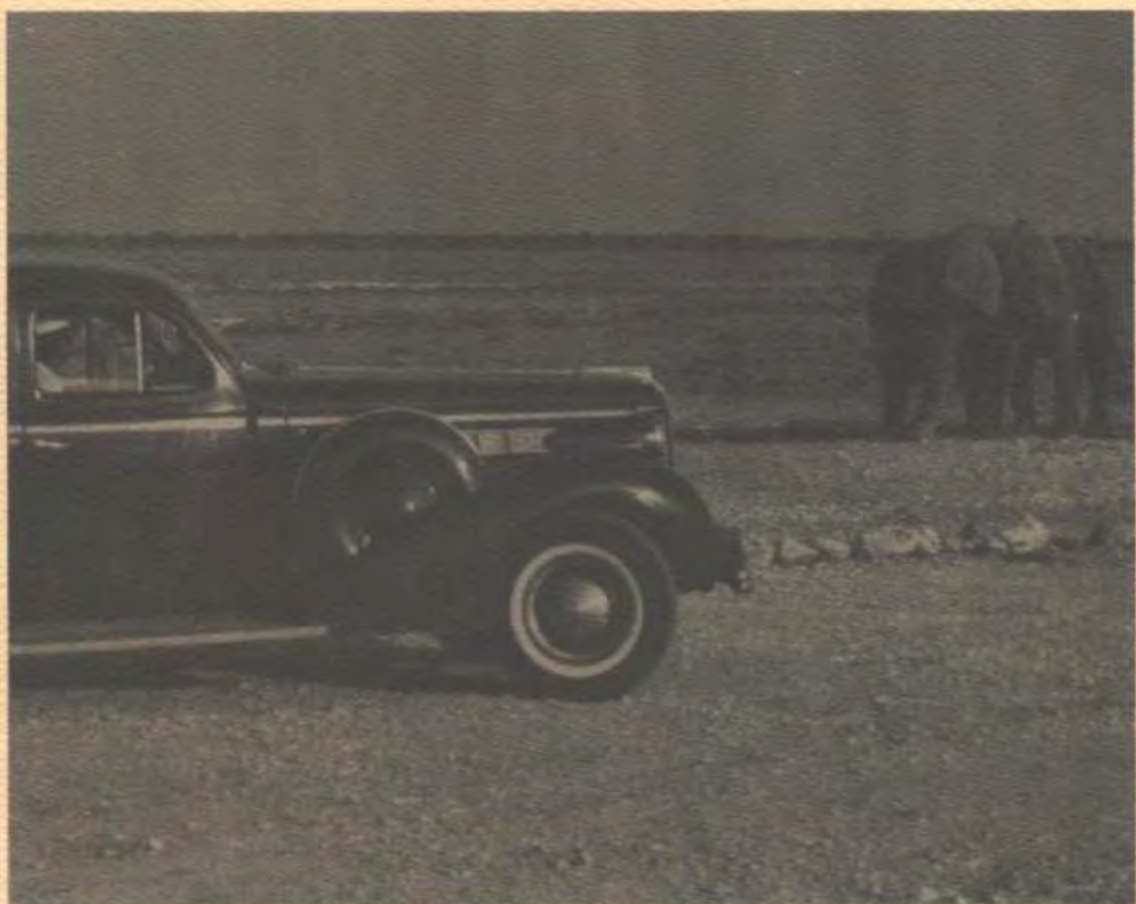
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≡ TORQUE TUBE ≡

THE NEWS PUBLICATION FOR MEMBERS OF THE 1937-1938 BUICK CLUB



VOLUME XI • NUMBER 4 • JANUARY / FEBRUARY • 1993

BUICK
1937  1938

TORQUE TUBE

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THE NEW TEAM



Putting faces to the names you have only read about until now, here is your new Torque Tube Team. Front row, left to right Dug Waggoner, Don Micheletti, Harry Logan and Jim Flack. Standing, George Canavan and Wayne Chan. Two representative vehicles of the club are from, left to right, Wayne Chan's 1937 Special 2 door trunk back sedan and Don Micheletti's 1938 Roadmaster sedan.



FOUNDED IN 1980 BY DAVE LEWIS



COVER CAR

1938 Century Model 61 sedan belonging to Peter Breitenstein (#989) from NAMIBIA in Africa. This photo was taken at a recent rally through Etosha National Game Park. The Buick had a leading role in a film called *THE SHELTERING DESERT*.

WELCOME TO VOLUME XI, NUMBER 4

By Wayne Chan #140

NO, THE TORQUE TUBE IS NOT DEAD!

Before we introduce ourselves, we all owe a great big "THANK YOU" to **BILL OLSON** for the tremendous job he has done in keeping the 1937-38 *Buick Club* viable with the wonderful publication and other administrative duties such as membership, etc., etc., for so many years. A truly thankless job with no end and really a monumental task done the "old fashioned" way as we have discovered. Once upon a time, I headed the Northern California Chapter of the BCA as either chairman or secretary for several years and suffered the same "old car burnout" that Professor Olson now states he has been afflicted with.....and my job was much smaller!

Bill's task has now been shouldered by Harry Logan (Editor), Dug Waggoner (Art Director), Jim Flack (Membership), George Canavan (Advertising), Don Micheletti (Technical Editor), and Wayne Chan (Contributing Editor). Bill Olson is also a contributing editor whenever he feels it's time to spice up our pages. And most important, this is an enthusiast publication for enthusiasts.....we are not professional writers. Please, if you have constructive criticism, suggestions, or better yet, 1937-38 Buick articles, stories, or pictures, write it down, send it in, and we'll publish it! At least in the near future, *The Torque Tube* will remain approximately as it has been, but a few of us plan to phase in new features

we think will be of interest to all the membership.

In addition to technical articles and stories about cars, we plan to profile our members, and as we possess a nice collection of historical archives which Bill has accumulated over the years and many

other items among the local members, we shall try to have flashbacks correlating advertisements, world events, etc., with the 1935-41 period. We may try eventually to feature "centerfold" cars! Another idea that was discussed was having regional reporters in various parts of the country reporting on cars and events elsewhere. Please give us some input or should you wish to volunteer your services, we welcome you with OPEN ARMS.



Bill Olson and Harry Logan sharing the warm California sunshine at the 1991 National Buick Meet in Sacramento, CA.

Now to introduce ourselves to you:



HARRY LOGAN, Editor. When I first met Harry, he was still the proud owner of 1938 Chevrolet business coupe--this was just like his first car, so he had to have one again! But, as with all auto enthusiasts, he soon got "itchy feet" and started looking at 1938 BUICK coupes. After looking at a number of cars, he decided on a "big block" 1938 66S which he drives proudly everywhere. Over a period of about five years, he has corrected virtually every flaw in the restoration

of the car he purchased. This man has a lot of energy and enthusiasm for old cars and now knows 1937-38 Buicks, bolt for bolt better than virtually anyone else in this area!

He is a retired video producer for Hewlett Packard here in Palo Alto, California. He is mighty handy with his camera and has a HUGE photo and literature collection relevant to 1937-38 Buicks as well as other old cars. He is also a collector of war memorabilia, magazines, etc. Folks, believe me, your editor is one great fellow and a tireless worker! With the memberships support and respect that comes with the job, Harry, as editor, will no doubt be a valuable resource for our great *Torque Tube*. He LOVES 1937-38 Buicks!

DUG WAGGONER, *Art Director*. Now this gentleman has a distinguished thirty year career applying his skills as an illustrator and graphic designer. He is undoubtedly going to play a very major part in the continued success and improvement of *The Torque Tube*. Dug has been well known for his artistic contributions to the publications of the Contemporary Historical Vehicle Association (CHVA): this has included the cover masthead for the national club magazine, the *Action Era Vehicle*, membership applications and the program cover paintings for their annual Saint Mary's Concours d'Elegance, two of which were reproduced on the covers of *Old Cars Weekly*. Other projects include creating ads for Dave Lewis plus ads and a fresh new catalog for Bob's Automobilia.

In the formative years of the *1937-38 Buick Club*, Dug was one the first members to join (#10) and support Dave's efforts with graphic images for the early *Swap'n Sell Newsletters*. The club shield, the *Torque Tube* Masthead, both old and new, are Dug's brainwork! He also worked with Bill Olson to create editorial/cover illustrations and a contest to change the moniker to the *Torque Tube* (contest winner credit goes to Bob Trueax, #351).

Thus, we have a very perceptive person in his business and in our old car hobby. We are extremely fortunate to have such an enthusiastic supporter as Dug. Yes, he does own a 1938 Buick model 44, two door sports sedan (slantback), which is in the midst of a complete restoration!

JIM FLACK, *Roster*. It was ten years ago or so that I first met Jim who was a member of the Northern California Buick Chapter. At that time, he had recently purchased a '37 46S and has since restored it to nice condition. Since we live close together, I frequently see Jim driving around the area in this car. Years ago, there was a Lincoln Continental Mark II in his possession.

Jim lives in a lovely vintage home built in 1937 and furnished in period furniture and antiques. He is intensely interested in the Art Deco era and is a member of the local Art Deco Society.

In recent months, Jim has been fortunate enough to acquire an all original '38 81F from a party in Southern California, so now with a '37 and a '38 he is a "two Buick man."

He is employed by Atari, a video company that designs visual methods for teaching peace officers techniques for high speed auto chases! His "high-tech" expertise and advice should greatly facilitate the maintenance of our club roster and modernization of our publication techniques for our *Torque Tube* magazine.

GEORGE CANAVAN, *Advertising*. He's another local old car person that doesn't let a few days go by that we wouldn't see George commuting in his '37 67. He is a do-it-yourself enthusiast. He also owns a lovely '65 Lincoln 4 door convertible. He has been into cars for years and considers "variety the spice of life." Each car he has owned has been a treatise on the technology of the time/country and on the quality of a given manufacturer: he has owned such cars as a '40 Ford commercial vehicle, '51 Ford business coupe, a very low mileage '56 DeSoto sedan, a vintage Citroen, Datsun Fairlady roadster, '50's Austin Healy, etc. He has had sports cars and has autocrossed and raced very quick automobiles.

Away from cars, George works in marketing management for a Fortune 500 corporation and over the years, his work has allowed him to travel extensively across the US and in Asia. He is active in the local Art Deco Society and promises Bill Olson that his 1937 Century will EVENTUALLY be Hampton Grey with grey Bedford Cord interior!

DON MICHELETTI, *Technical Editor*. For about 15 years or so I have known Don. We first met in the Northern California Buick Chapter and went on many a tour together in the late 1970's and early 80's. He is another "hands on" old car man with a tremendous mechanical aptitude. He does virtually all his own work and realizes exactly what needs to be done. He owns a 1938 46S, a 1938 81, 1918 Buick touring car, and has owned in the past, 1910, 1931, 1932 Buicks, and a 1957 Olds "88." In keeping with his mechanical side, he also belongs to the local railroad society that restores 1:1 scale locomotives. The man has steam in his blood. He is a mechanical engineer with Raychem. A tremendous friend of old cars!

WAYNE CHAN, *Contributing Editor*. My love for 1937-38 Buicks goes back to age 3 when I first realized what a car was. My father owned a 1937 47 during and immediately after World War II (total of seven years) and I knew our old car well.....it was really a friend. I drew pictures of 1937-38 Buicks, their speedometers,

dashes, etc. for entertainment.....talk about a "one track mind:" (my mother's description)!

I collect old auto literature with specialty being BUICK, of all makes! The house also has about 3-4000 miniature cars and several one off 1937-38 Buicks in scale. I am a past chairman/secretary of the Northern California Chapter of BCA in the early 1980's. I have owned several old cars: 1950 Olds "88," 1941 Buick 41 SE, 1941 46S, but have since 1977 devoted my attention and care to the restoration and maintenance of a 78,000 original mile 1937 48 that is my pride and joy. OCCUPATION: Physician (Dermatologist) in private practice. Other hobbies: love to cook and dabble in wine collecting/tasting and classical music too.

So, there you have all the various backgrounds of the new Torque Tube Team. We hope you continue to enjoy the 1937-38 Buick Club, your cars, and this publication as it continues "under new management." To me, 1937-1938 Buicks will always be **NUMERO UNO!** May 1993 be happy and prosperous for all of you.

TORQUE TUBE

The **TORQUE TUBE** is published bi-monthly for the enjoyment of the 1937-1938 Buick Club. Membership dues are \$30.00 per year US, \$31.00 (US) per year Canada; for overseas and multi-year rates please contact the editor. All memberships run from September 1 to August 31 of the next year. Dues are not prorated; however, persons joining during a membership year prior to June 1 receive all issues since the preceding annual starting date of September 1.

Please send all articles, ads subscriptions and inquires, etc. to:

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'37-'38 EAST & WEST COAST MEETS

EAST COAST MEET....*Lou Wildt (#245)* and *Rich Dickman (#765)* are considering putting together a '37-'38 meet in the Ohio Tri-State area.

They are going to check it out and let us know and we'll publish it in the *Torque Tube*.

WEST COAST MEET...*Bill Schaeffer (#622)* and I are planning on piggy-backing a '37-'38 short tour at the end of the Yosemite All GM Fun Run May 20 to May 23. This would be the morning of Sunday May 23. Most of the members will be staying in the Fish Camp/Oakhurst area.

We would leave the Tenaya Lodge in Fish Camp where the meet is being held around 10 AM and caravan to Mariposa where we would have breakfast at the Miner's Inn. There is also the possibility of visiting a wrecking yard in the area with cars from the 40's up. There will be more

details in your next issue of the *Torque Tube*.

The GM Fun Run is sponsored by the Capitol Chapter of the BCA and *Carl and Judy Dahl (#868)* are helping to put it on. There will be a tour of Bass Lake including lunch and a ride on the Sugar Pine Railroad on Friday May 21. A Yosemite tour is planned for Saturday as well as supper at the Lodge. Contact the Dahls at (916) 441-2507 to register.

The Meet will be at the Tenaya Lodge in Fish Camp (800) 635-5807. \$119/per night plus 9% tax. There are other motel/hotels at around \$85/\$90 such as the Narrow Gauge Inn 2 miles away. The number is (209) 683-7720. There are also some more Inns in nearby Oakhurst.

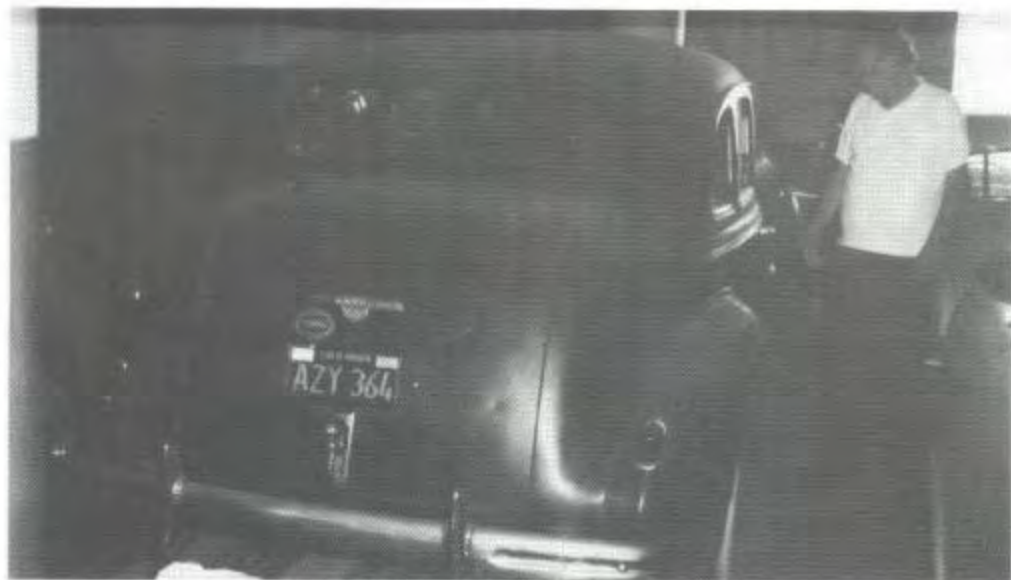
The key point is that May is the busy season in Yosemite, so get your hotel/motel reservations in now. You can always change later if you have to.

CENTURY DISCOVERED IN WAREHOUSE

By Harry Logan, Editor



Viewing and photographing the engine of the '38 Century sedan by Curt Brohard in the tight confines of the warehouse made for a difficult task.



'38-61 with owner Bill Starr.

Curt Brohard (#554) phoned to ask me if I would go with him to look at a '38 Century 4 door sedan for sale. It along with 52 other cars from 1914 Model T's, 20's Buick sedan, '30 LaSalle, '39 Buick convertible, '41 Cadillac convertible etc. were stored in a warehouse in Oakland, CA.

Curt and I met Bill Starr, the owner at the warehouse. All the cars were packed tightly together with a small aisle between them.

About half way through the warehouse, we spotted a Gainsborough Blue '38-61. The paint was

original as was the rest of the car. The original upholstery was protected with seat covers and was in good condition as was the woodgraining.

The car had a 1960's license plates and had not been driven in years. There was no rust anywhere. The trunk contained a box of new PortaWalls, used to make whitewalls out of blackwall tires.

The car needed a total restoration. Unfortunately, the owner must have had eyesight as he seems to believe it's already restored based on the \$15,000 firm he's asking. Thanks but no thanks!



Engine compartment with original radiator, air cleaner and Stromberg AAV-2 carburetor. The hose running over the valve cover goes to the windshield wiper. Note the car is late '38 as indicated by the lack of a center stud on the valve cover. Buick switched from 3 to 2 studs at S/N 3525209 for Series 60-80-90 and S/N 4-3534043 for Series 40 cars.

KOMPARISON KORNER

By Harry Logan, Editor

Being an analytical type person and a history buff, I enjoy comparing things. I lived in England and Belgium for 5 years when I worked as a Field Engineer for Hoffman Electronics Corp. in the 60's. I would go to historical sites (I used to pass Waterloo every day on the way to work when I lived in Belgium) with pictures of what it looked like years before and compare the changes.

I had a chance to do this again last year when I visited Dave Lewis in Springfield, Illinois, Abraham Lincoln's hometown. I had some old magazines with engravings of Lincoln's burial. I went to the cemetery and compared his original tomb (he's now interred with his family in a monument a short distance away) to the engravings.

There's an interesting magazine called "After the Battle" that features articles on WWII events with "now and then" photos.

These things inspired me to have a column in each issue of the *Torque Tube* comparing various aspects of our Buicks. I mentioned this idea to Charles Jekofsky #524 who has sent me some photos of his fathers '38-66S taken in 1941 and photos of his 38-61 taken in 1993 at the same Washington, D.C. locations.....a 52 year time span. (Look for it in the next issue).

To begin our comparisons between our two vehicles, let's look at '37 and '38 hood louver model name emblems. The differences between the two are rather obvious but might surprise you.



'37 emblem is from an unrestored car. The emblem is chrome plated brass. The background is silver. The raised letters are chromed. The louvers were originally silver but have turned brown over the years due to surface rust. This is usually the case with unrestored cars. (The Buick is George Canavan's 37-67).



'38 emblem. The louvers are painted silver. The emblem is again chrome plated brass. The edges of the letters are left chromed while the insides are painted red. The background is silver like the louvers. (The Buick is Charles Jekofsky's 38-61).



To restore an emblem, first have it chromed and then paint it. For a '37, paint the background silver to match the hood louvers. Many '37 owners paint the background black or the body color. This makes the letters stand out but is not the way they were originally when they left the factory.



A wintery scene in Connecticut and a nice looking Raphael green '38 Century Convertible. The car belonging to Classic Car Sales. I hope they take the car in at night. How much snow can that top hold?

SAVE THOSE OLD ISSUES OF THE TORQUE TUBE

By Saul Hoffman #666

Last August at the ALL GM MEET in Santa Maria, Ca., I purchased my bumper badge from a local person who had noted the hole in the front bumper of my '38 Business coupe. The badge was in fairly good shape but overchromed and poorly hand painted ...but the price was right.

Last Sunday after the football games, I decided to tackle a small job and chose to repaint the bumper badge. The previous owner had used a blue paint for the "8" and stripe, and painted the raised B-U-I-C-K letters blue. This didn't look right to me. I removed all of the blue paint, looked at the shiny badge and thought how the hell do I paint it?" There wasn't very good definition between the raised letters and the background because of the heavy layer of chrome. I knew that Harry Logan had a bumper badge on his '38 Century coupe, but he lives an hour from me, and since it was raining like hell, I wasn't about to put on the old galoshes and hike over to Harry's place in Los Altos.

I went to the bookcase where I keep my old car magazines and pulled out the pile of Torque Tubes. Lo and behold, right on the cover of Volume IX, Number 4 (January, 1991) was a picture of the nose end of a '38, high-lighting a bumper badge. It was there I noticed that the raised BUICK letters were not painted, but instead the background was painted....sort of in bas relief. The photo was good enough to show the detail of a separation between the lower part of the eight and the solid stripe that

goes through that part of the eight.

I used Dupli-Color Touch-Up paint, SF GM 398, GM Code 81/WA 8774, which is described as General Motors Corp. Bright Red Clearcoat 1985-1990. (Yeah, I know, it's supposed to be black, but

my car came with red wheels, red painted hub caps and red bumper guard stripes which look real sharp, and I ain't changing the color to black). The touch-up paint dries very quickly, so you have to work carefully. An Exacto knife was used to remove excess paint.

So save those back issues of *The Torque Tube*. They may come in handy, even for little jobs.

(Editors Note) If you look closely at '38 bumper badges at car meets, you'll see various combinations of red and black. Paul Cusano #052 who has researched this believes the "BUICK" and the diagonal line are RED and the "8" is BLACK.

Several years ago, I purchased an original dinged-up badge showing some age as you might guess but with some of paint still left on it. The red and black were just as Paul believes.

Again in keeping with the original premise of this article, if you dig out your old *Torque Tubes* notice the photo in Volume XI, Number 1, page 6. It was taken in 1941 and shows Charles Jekofsky's #524 father's '38 Century coupe. The bumper badge "8" is darker than the diagonal or BUICK, indicating it is black. Again, agreement with Paul Cusano. So that's the way I painted my badge.



Cecil's Century

By Harry Logan

CECIL DON #637 has loved Buicks since the age of six when his mother got a '31 4 door sedan. He has lived in Yuba City, near Sacramento all his life. He is one of the owners of a thriving trucking business and also the director for the *Capitol Chapter of the Buick Club of America*.

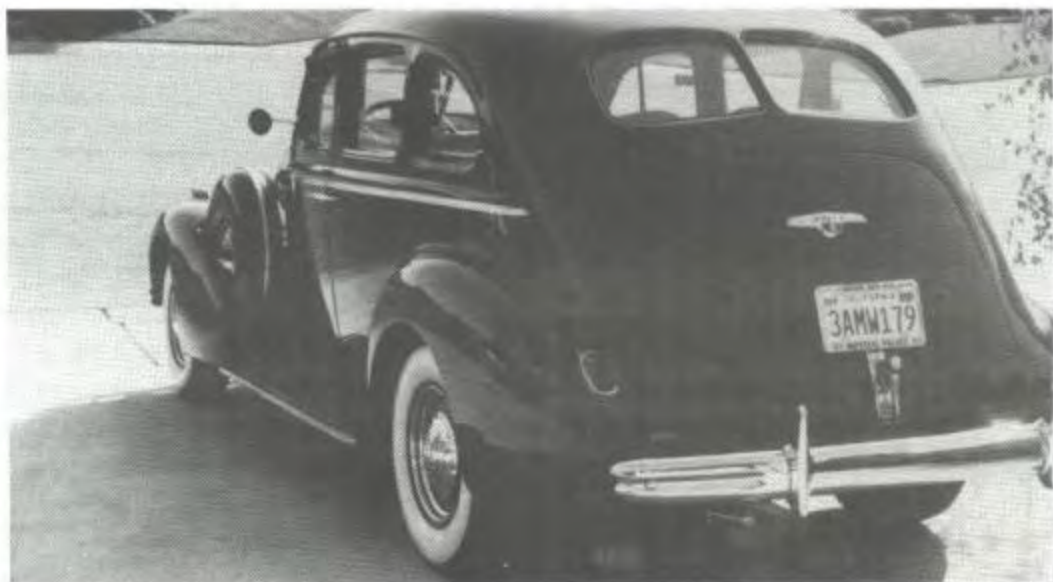
When I first met Cecil several years ago, he had a '37 Special sedan which he took on many tours. He also has a '37 Limited Model 91. But since

the day I first met him, he's always wanted a '37 or '38 Century. In fact the first thing he ever said to me was: "Do you know of a good Century for sale!"

While at the West Coast Buick Meet last August in Washington state, he spotted a '38-67. It was love at first sight. The car was not for sale but Cecil, known for his persuasive techniques, was able to convince the owner to part with it and he now has his dream car. Man of the Century.



The Black Beauty, a long nosed '38 Century.



Rare '38 slant back Century sedan. The slantback styling made a more graceful body line but the trunk backs were much more popular, probably because they held more luggage.



Most '38 Century's had a horizontal grained reddish mahogany dashboard with 2 stainless trim strips, one at each end (Some Century's had the chevron pattern instead). You can see one of the trim strips between the glove box door and ash tray. '38 Specials do not have these trim strips and also have different woodgraining.



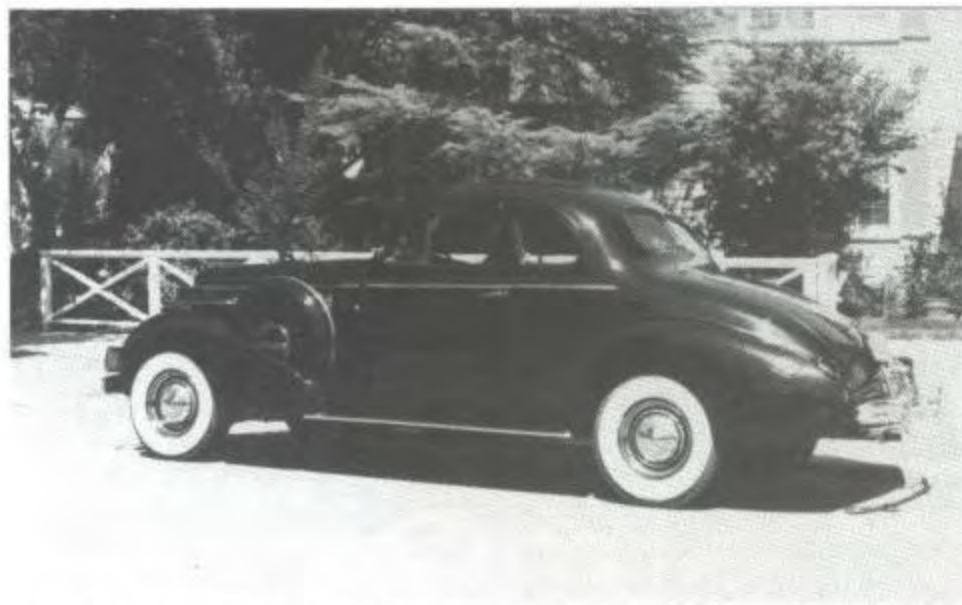
The plush rear seating area for Century has a fold out arm rest in the middle plus stainless trim strips attached to the bottom of the window garnish mouldings. The Specials, although quite comfortable lacked these amenities.

1938 SPORTS COUPES

2 COAST-TO-COAST



Very nice original condition (including the paint) black 38-665 belonging to Larry Tindall (#892) of Highstown, N.J. It was featured in Vol. V, No. 2. of the Torque Tube when it was owned by Rocky Farano, #276.



And a nearly identical shot of Harry Logan's maroon 665 in Los Altos, CA. Now how's that for two '38 Century Sport Coupe look-a-likes a continent apart.

The Wedding March

By Tom Kamide #803



Janine and I met in January of 1990 through a mutual friend. I am 37 years YOUNG, a CPA and a partner in a firm here in Syracuse, N.Y.

Janine, on the other hand, is 28 years OLD and is a CPA working for a local CPA firm.

As the months passed by and after spending a significant amount of time with Janine, I found myself beginning to relent to the sting of the Love Bug. At first, I thought I could never marry a bean counter (CPA) since I was one myself. However, her beauty and lovely personality finally swept me, a bachelor of 37 years, off my feet.

While dating Janine we often went out for weekend joy rides and picnics in my '38 Buick Sport Coupe. Wherever we drove, we were sure to draw

attention with that beautiful old car. On a few of our Sunday excursions I even let her drive, something I never dreamed I would do, but her beauty was always my "weakness". We were engaged Christmas of 1991 and set the wedding for October of 1992. Throughout 1992 we were very busy with all the preparation and planning for the big event.

I purchased my '38-46S in October of 1988 from Anthony Genito in Carthage, N.Y. (my home town) 85 miles north of Syracuse. The two prior owners of this fine automobile lived on the same block in Carthage and so did I. Imagine an automobile 55 years old and all of its owners have lived on the same block!

I'll bet that doesn't happen often.



Since owning the car I've made numerous improvements to her yearly. This years projects were to rechrome 90% of the car, replace the rear wiring harness, recast the steering wheel, sandblast, repaint and resripe the wheel rims, and replace the interior with a new one from Hampton Coach and it turned out great. I could not believe it was the same automobile. In September of 1992 I was fortunate enough to purchase my third Buick, a 1937 Model 61 from Robert Lockwood in Las Vegas, Nevada. Bob is also a '37/'38 Club member and the car was advertised in the September, 1992 Torque Tube. I was extremely happy with the car. As you can see, she is a beauty. But I'll save that story for another day. (Ed. And you'll see it here).

The week before the wedding was crazy, so many last minute tasks that needed to be accomplished. At times it was very hectic and like a good groom, I concentrated on what I thought was important like spending the day before the wedding waxing all my cars. I'm sure that my misdirection often caused my Cinderella to transform into the Wicked Witch of the West.

The wedding was at 10 AM, so everyone was up bright and early. I arrived at my father-in-law's garage at about 8:30 AM to make sure the cars were running properly. We were all hoping that Friday's weather would continue through Saturday, sunny and 70 degrees, but no such luck. It was cloudy and about 60 degrees, not bad for the end of



Tom Kamide (left) and his best man with his recently purchased 37-61. It came with Senior Trippie lights and a '38 dinner gong grille guard which Tom plans to put on his '38 coupe. (A much better looking accessory than the after-market variety).

October in Central New York. We just hoped the approaching rain clouds would keep their distance until after the wedding.

At about 9 AM, we fired up the cars. The wedding procession of cars consisted of my '38-46S, '37-61, and my mother-in-laws 1950 Packard Clipper, my father-in-laws 1967 Buick Le Sabre Coupe and a 1989 Jaguar. Upon leaving the garage with the Buicks, we headed for my in-laws house to pick up the remaining cars and take some pictures. All that took about 30 minutes and all the ladies started screaming for us to get going to the church since it was a ways away. Again we fired up the cars and along we went to St. Augustines church in Baldwinsville, N.Y. This bachelor of 37 years would be no more!

We made it to the church on time and began to greet the guests and complete all the other necessary tasks before the start of the wedding. Well, moments later, the bride arrived and I disappeared so as not to see the "Princess" before her walk down the aisle. The wedding was over in an hour, and just before everyone was about to leave the church, those distant rain clouds decided to stop overhead and the rain really began to fall much to everyone's dismay. All that waxing and detailing

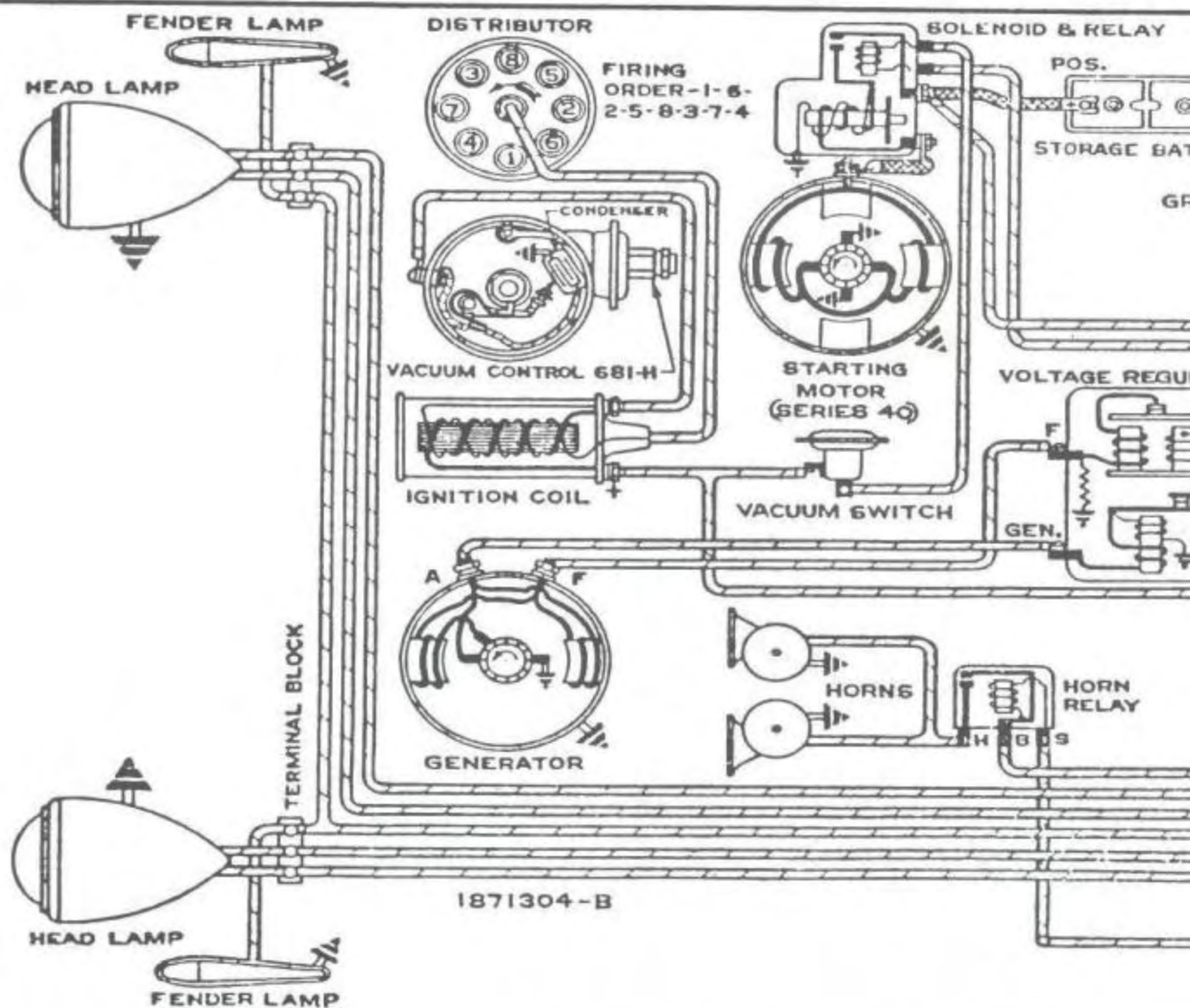
just for those rain drops! Well, some people say rain is a sign of "Good Luck" for the future, so we should have plenty!

After the ceremony, we greeted all our 375 guests and then off to the reception at the Hotel Syracuse. Everything went as planned and everyone had a splendid time. The only time I felt a little ill was when I had to write that enormous check. "All that for one day!!", I said to myself. I pulled out my trusty pen, broke out the checkbook, and like a good accountant began to let go of some of my baptismal money.

At the conclusion of the reception everyone was invited to an after wedding party at my in-laws. The party lasted past midnight but as a good bride and groom we dashed away to the Honeymoon Suite to partake in the Honeymoon festivities. The next day at 1:00 PM we began our departure for sunny Florida's Walt Disney World in Orlando. We had a spectacular time and would recommend a stay to anyone. The eight days went by fast and we were back home....and back to work. More debits and credits and the beginning of the year end tax planning season. I am sure that as we pursue our road through life, we'll always include our "Good Ole Buicks" in its joys and pleasures.



With a smiling flower girl on the jump seat the Bride goes to the reception.

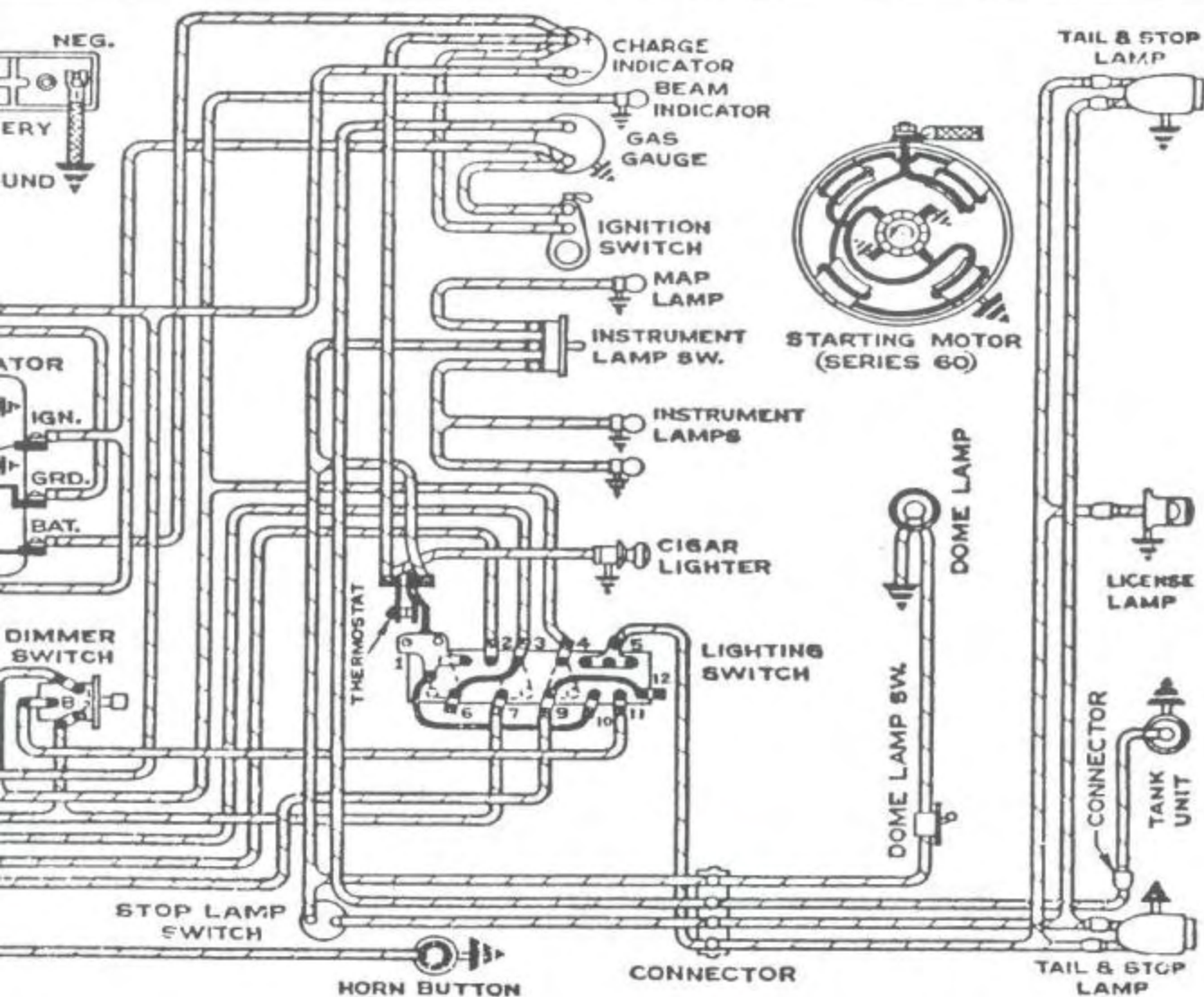


'38 BUICK SERIES 40 AND 60 WIRING DIAGRAM

This enlarged wiring diagram is much easier to follow than the ones from Buick that are in the Owners and Service Manuals. These are after-market diagrams for use by mechanics.

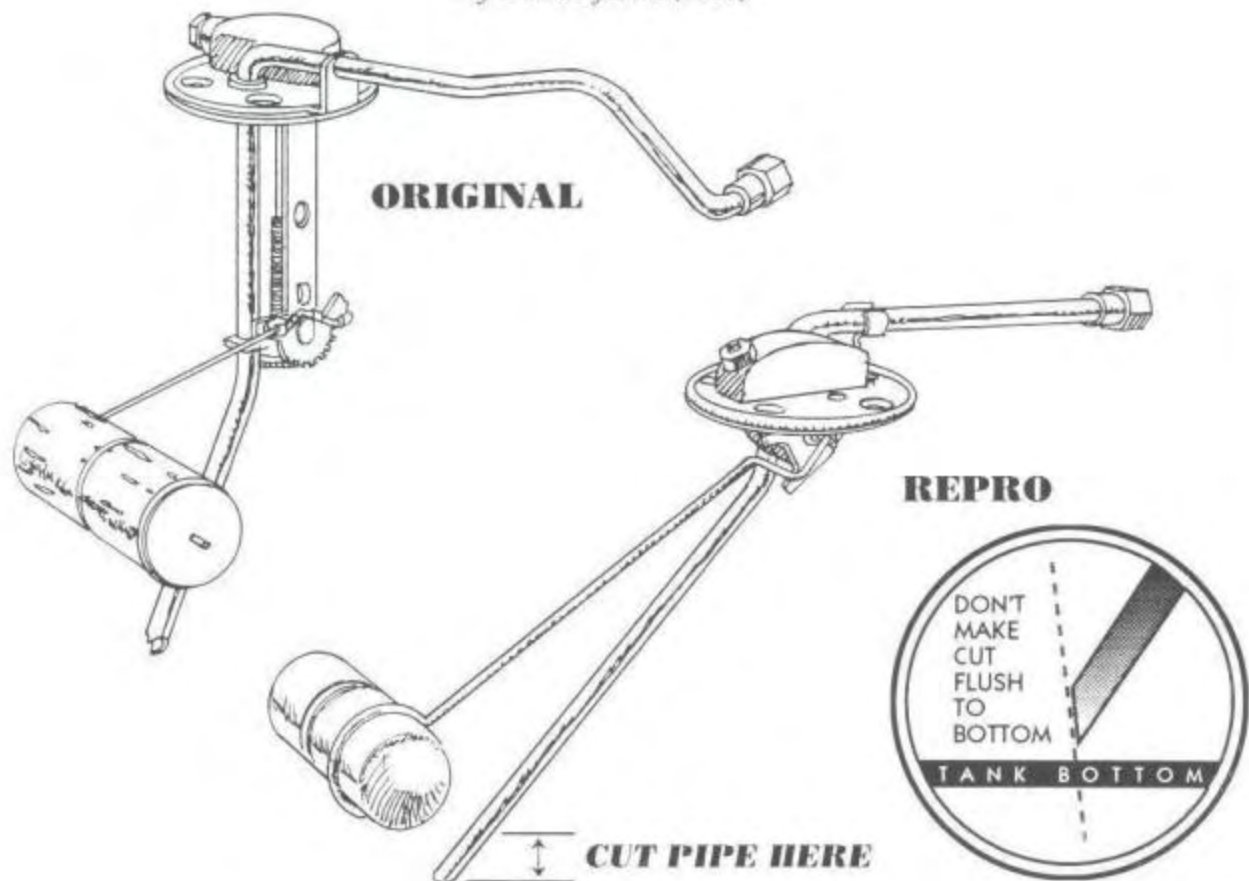
I used it to help install a new wiring harness. After installing a wire, I used a red pencil to color in the wire on the diagram. That made it easy to see my progress and helped prevent any errors.

The '38 Series 80 and 90 wiring diagrams are identical except for the door switches, dome light and two cigar lighters in the rear seat. I believe it can also be used for '37 Buicks.



FIXING YOUR GAS TANK SENDING UNIT

By David Bylsma (#117)



I checked out the repro gas gauge sending unit from Bob's Automobilia. It's listed as a 1939 - 1955 sending unit. CARS lists one as a 1941 - 1956 sending unit. I checked the top of the unit. It will bolt on to the gas tank with no modification other than the gas pipe that goes into the tank. It is a bit longer than the original, so you'll have to cut the pipe in order for the unit to fit in the tank.

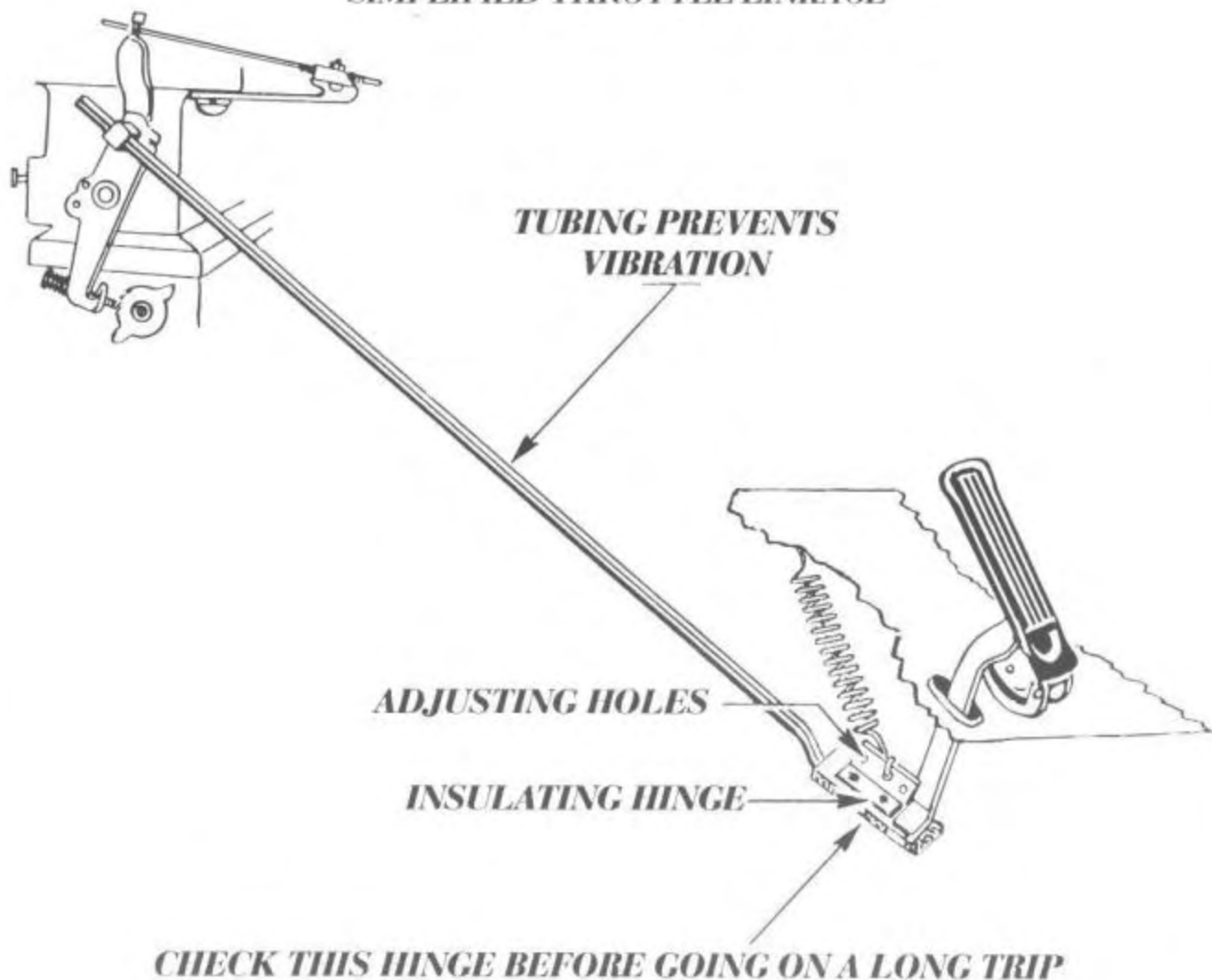
It worked on my 1938 Century, so I would think it will fit all '37 and '38 Buicks.

(Editors Note) My '38 Century runs out of gas when the gauge reads one-quarter full. The gauge is OK, so the problem is in the sending unit. After looking at my original sending unit I might add that you should take into consideration that the original "gas pickup pipe" inside the tank was cut in such a way that it was NOT flush with the bottom of the tank. (See inset) Also the repro sending unit "float arm" might have to be altered to fit. I just ran out of gas for the second time in 5 years because of this, so I can really use this tip. Thanks David.

'38 BUICK PREVENTATIVE MAINTENANCE

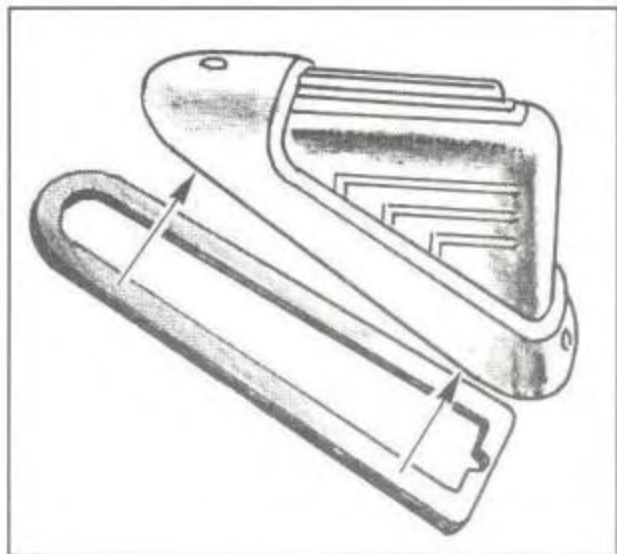
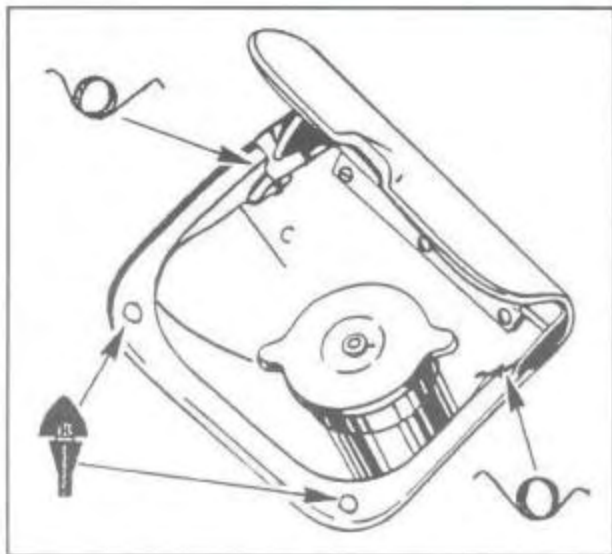
By Hugh Patterson (#300)

SIMPLIFIED THROTTLE LINKAGE



This is an item that is often overlooked. When mine broke, I was fortunate to be only one mile from home and learned how to drive with the hand throttle. This piece rots out over time and is very easy to replace while you're at home and very difficult when you're on the road with few tools.
(Reprinted from Vol. II, No. 7)

'38 GAS FILLER DOOR & TAIL LIGHT LENS



When I bought my '38-66S five years ago, the gas tank filler door would not close completely because the two springs that keep it closed were missing as were the two rubber bumpers.

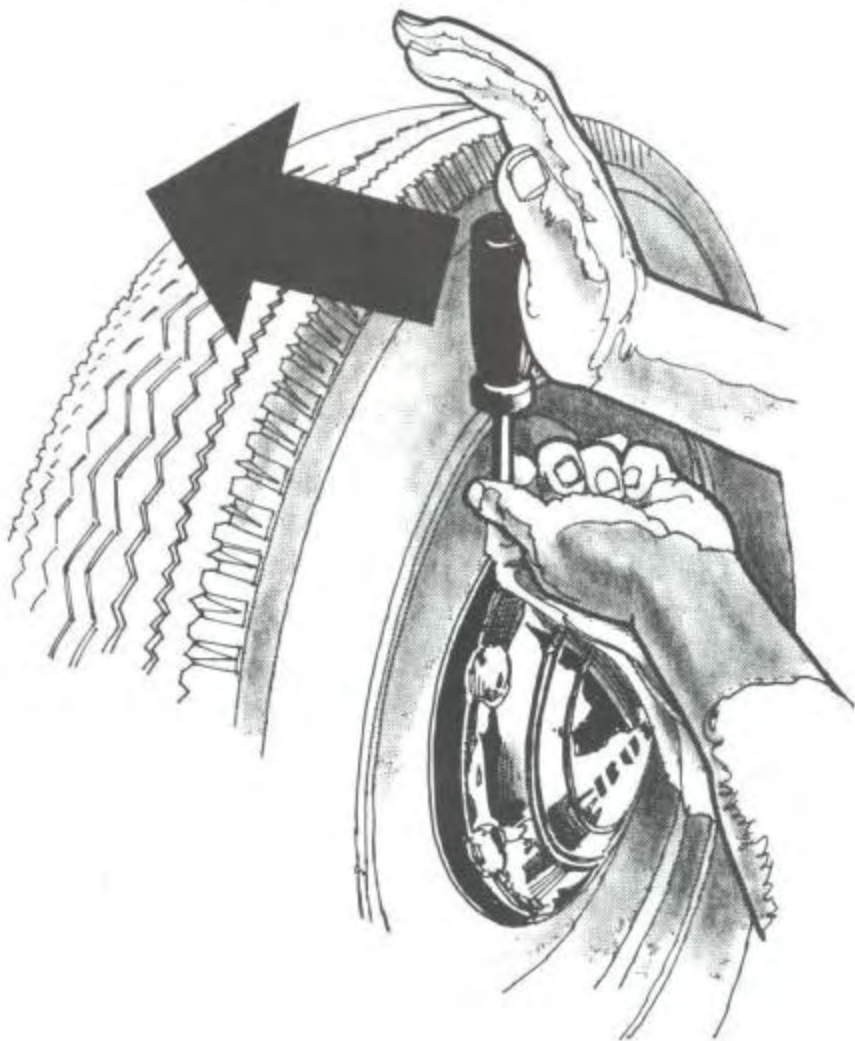
In addition, the red tail light lenses were loose because the cork gaskets that prevent water getting in were missing. All these parts are now available from Bob's Automobilia and other dealers.

QUESTIONS & ANSWERS

♦ In the last issue of the Torque Tube on page 25, there was a question about putting a '51 Super 263 CID engine into a '38. Lou Wildt #245 says the answer is MAYBE. If the engine is from a Dynaflo car, it would require using a crankshaft from a standard shift car. He believes the end of the crankshaft is different between the standard and Dynaflo and a standard flywheel will not fit the Dynaflo crankshaft. His Hollander interchange manual lists '37 - '52 (40,50) except Dynaflo. Lou will check this out at the next BCA chapter meeting where they have a library which contains a master parts list. (Ed. Thanks Lou)



REMOVING HUBCAPS



Dave Lewis says the best way to remove hubcaps without scratching the painted rim is to wrap the tip of a long (12 inches or so) screwdriver with a rag, insert it between the hubcap and rim, then give it a quick hit with the palm of your hand. Be sure to have a helper holding on to the hubcap so it doesn't fall on the ground. Dave says that when he's working alone, he uses a blanket to catch the hubcap.

I tried this and it works. I found it worked best if I tapped the handle end of the screwdriver first to get the rag protected blade firmly down in between the hubcap and the rim before giving it that last hit with the palm of your hand. Make sure the rag also covers the screwdriver shaft where it touches the beauty rim. Sometimes the hubcaps really fly out, so have the blanket spread out several feet from the rim.



STARTER SWITCHES

My car like many Buicks had a push button switch when I bought it. After installing the correct Stromberg AAV-2 carburetor with a vacuum switch, I connected the two wires that went to the push button starter switch to the vacuum switch on the carburetor. So now the car starts using the accelerator pedal, just like it did when it left the factory.

The vacuum switches were changed to push button ones over the years because they are not as reliable. I checked the wiring diagram and found I that by running two wires to the push button, I could keep both methods of starting my car. These two wires place the push button switch in parallel with the vacuum switch.

I ran one wire from the gas gauge (the side that connects to the ignition switch) and the other from the starter solenoid relay (the side that goes to the vacuum switch...use an ohmmeter to make sure you've got the correct wire). This is the only extra wire you can see. It runs up the firewall from the starter solenoid, but I placed it under oil pressure gauge tube, windshield wiper tube and water temperature gauge wire that also run up the firewall above the starter. This hides it well.

Now my car starts with either the push button under the dash or the accelerator pedal. Normally I use the accelerator pedal, but once it failed and I was sure glad I had the push button to fall back on.

STRANGE NOISES

Last August I took my 38-66S on a 500 mile trip to the All GM Meet in Santa Maria, Ca. with no problems. But shortly after returning home, I began to hear a squeaking sound from the front end. The more I drove, the worse the noise became. It was hard to pin down exactly where it was coming from, but it seemed to be from the front on the drivers side.

I raised the car, crawled under and visually check everything. I found a couple of loose bolts that I tightened, but the noise was still there. Then I removed the left front wheel and check the wheel bearings. That was it. Some of the balls in the outer bearing were badly scored as was the race.

After installing new bearings and races on the left front wheel, the noise disappeared.

USEFUL INFORMATION

In the last issue, Jack Shepherd described his 1951 trip from Vancouver to Montreal in his '38 Buick. He mentioned having to stop every 20 miles to regap a sparkplug as the electrodes would close. The problem was due to part of the ridge that holds the top piston ring in place breaking off. The small metal piece bounced around in the cylinder and hit the spark plug electrode and eventual closed it. The mechanic removed the metal piece by placing the piston at top-dead-center and blowing it out with compressed air.

This is the fourth Buick I've heard of with this problem. Jim Flack has this problem now on his '38-81F. A friend in Southern California told me his father's '38 had this problem on all cylinders back in 1940. Another friend has this problem now on three cylinders of his '39 Buick. So if you run into this unusual problem you'll know what's wrong.

LICENSE PLATE RESTORATION

Over the years, I've had several sets of license plates restored. The best job was done by Darryl's of St. Petersburg. He recently did a set of '37 and '38 plates for me and they are outstanding. He charges \$60 per set plus shipping. His address is:

Darryl's
4505 8th Ave. South
St. Petersburg, Florida 33711
(813) 321-8279

CAR WAX RECOMMENDATION

The wax Tom Kamide used to polish all the old cars in his wedding is a product made in Utica, N.Y. It's called Collinite No. 4765. Tom says he's tried many waxes and has never found one that can match the brilliance, durability and luster of Collinite. Tom says to make sure you also order the pre-wax cleaner No. 840 that is used to clean the car before waxing. You won't be disappointed!

If anyone is interested, here is where to contact them.

Collinite Corporation
1520 Lincoln Avenue
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(315) 732-2282

STAINLESS TRIM RESTORATION

I've recently had several pieces of stainless trim restored by Dave Turner for my '37-66S. He was recommended to me by Charles Van Koten of Napa, Ca. who has restored several '37 Buicks. Even Dave Lewis who is a pretty fussy guy was pleased with his workmanship. His address is:

Dave Turner
4388 South Willow
Fresno, CA 93725
(209) 237-0892

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*Paul Culp, Jr. #508
285 Elephant Rd.
Perkasie, PA 18944
(215) 249-3166
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Peter Breitenstein #989
P.O. Box 2805
Windhoek, NAMIBIA, AFRICA
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Leo McNamara #990
257 Elmhurst Ave.
Elmhurst, IL 60126
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Darcy Ellison #991
525 18A Street NW
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**old members who re-subscribed.*

EDITORS NOTE

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- ◆ '37 Century 15" **Wheels**

- ◆ '37 **Steering Column** with **Wheel** needing restoration.

Lots of small parts. Call or write for your needs. I just might have it.

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- ◆ '37 Series 40 rebuilt **Transmission**. New bearings, bushings, thrust washers, reverse idler gear, countershaft, universal joint, torque ball seal kit.

All clearances put back to factory specs.....\$275.00 plus shipping.

- ◆ Re-manufactured **Clutch Disc** for '37 40 Series.....\$ 39.64 plus shipping.

David Willard #988

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(612) 731-3283



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- ◆ One pair of 1937 40 series **Sidemount Fenders**. The fenders include all of the attaching hardware (fender frame braces, spare tire supports, nuts and home made lock plates). **No covers**. The fenders are rough but solid with surface rust. **\$750.00 obo.**
- ◆ One right side **Sidemount Cover** for 1937/1938 60 series. All stainless is nice, name plate is well worn in center from rubbing the fender, face plate is rusty with a small spot of rust out in top..... **\$175.00 obo**
- ◆ One left side **Sidemount Tread Cover** with inside cover plate, nice stainless steel, fits 1937/1938 40 series..... **\$ 50.00.**
- ◆ Two **Sidemount Lock Plates**, nice, not 40 series. They fit holes which are at least 3-3/8 inches in diameter..... **\$ 25.00 each.**
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Prices do not include shipping. I prefer not to ship the fenders.

Kieth Ladderud #163
21708 SE 291st Street
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(206) 85-BUICK

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- ◆ One **Gas Pedal** - Needs rubber..... **\$ 2.00**
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No hardware but has trim ring on tread covers..... **\$700.00 FIRM**

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FOR '38 Century - 61 (*New Member and owner of the COVER CAR*)

- ◆ Two (left and right) **Stainless Trim Rings** and **BUICK** nameplate for sidemount covers. (*You can see they're missing in the cover photo*).
- ◆ One **Stainless Trim Strip** for the left (carburetor) side of the hood.
- ◆ **Clock** for glove box door.

Peter Breitenstein #989

P.O. Box 2805

Windhoek, NAMIBIA, AFRICA

FOR '37 Century

- ◆ 37 Century rebuilt or core **Rear Shock Absorbers**. Have front shock absorber cores for trade.

George Canavan #806

1035 Robin Hood Ct.

Los Altos, CA 94022

(415) 967-4909

- ◆ **WANTED....Repro Center Stainless Hood Strip** for '37/'38 Big Series cars. Does anyone have one they are not using? Must be 45 inches long. Thanks.

Harry Logan #651

1005 Rilma Lane

Los Altos, CA 94022

(415) 941-4587

- ◆ **ALL OF THESE ITEMS WANTED...Footrest** for 1937 Series 90, **Back seat, 2 Back Seat Cigarette Lighters, Right Sun Visor.**

Bruce Barrett #836

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'38 Buick Century, very rare Model 67 Sport Sedan. Only 1515 produced, now one of only 5 known to exist per registries of the '37-'38 Buick Club and the BSA. Completely restored California original, grey with a beautiful blue leather interior, dual sidemounts, rebuilt engine with only 3,000 miles plus recently rebuilt transmission and clutch. Ready to drive anywhere there's a road. Photos Available.

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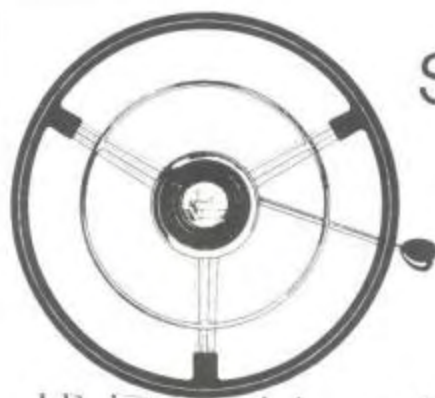
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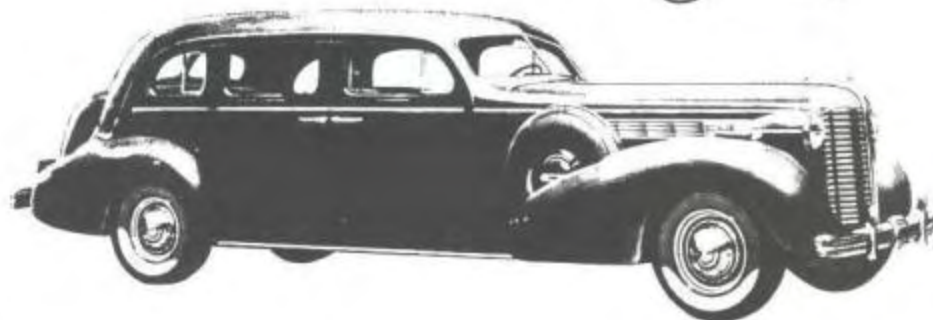
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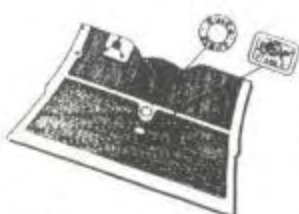
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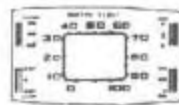
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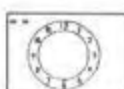
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